

Application Number	Date of Appln	Committee Date	Ward
125474/FO/2019	8th Jan 2020	13th Feb 2020	Clayton & Openshaw Ward

Proposal Proposed erection of 23 no. two storey dwellinghouses with associated car parking, hard and soft landscaping and boundary treatments.

Location Land Adjacent to Mayton Street, Manchester, M11 2AN

Applicant Ms Victoria Young , One Manchester, Lovell House, 6 Archway, Manchester, M15 5RN,

Agent Mr Daniel Tomlinson, GWP Architecture, 1 Lidgett Lane, Leeds, LS8 1PQ

Description

This application has been submitted by One Manchester as part of an overall housing strategy for the area. The development would comprise 23 x two storey houses with a mix of two and three bedrooms. The applicant would deliver and manage the proposed houses and upgraded open space within this previously developed brownfield site. The site has been identified as being potential suitable for residential development in the 2019 Strategic Housing Land Availability Assessment (SHLAA). In addition to its contribution to family housing, the development would also deliver new street lighting, tree planting and seating within a retained area of landscaped open space.

The application site comprises of land previously developed for housing that has been maintained as open green space. It is intersected by a network of footpaths with incidental mature tree planting and surrounded highways along Beede Street, Mayton Street, Herne Street, Newtown Close and Thames Close, which also provide access to neighbouring housing and communal on street car parking bays. The surrounding area mainly comprises of two storey pitched roof terraced housing with semi-detached houses located to the north of the site along Mayton Street and Beede Street. A mix of single, two and three storey houses and apartments are located to the south of the site. Although the area is predominantly residential, there is a 3-storey telephone exchange building located to the north-west of the site.

The proposed development would comprise 23 houses that would present their principal frontage to the existing streetscene formed along Beede Street, Mayton Street, Herne Street and Thames Close. The proposed houses would be arranged in 3 unit terraces and semi-detached pairs. To the south of the site a new access road would be formed to the south of the adjacent to 47 Herne Street and an existing and retained play area. Proposed houses would also front the proposed access road and would overlook communal car parking bays (10 spaces in total). An area of communal car parking would also be formed adjacent to Thames Close to the east of the site. Footpaths would be formed alongside the new access road would extended into an area of retained open space that would provide a setting to the

adjacent proposed housing. Permeable routes would be formed across the open spaces to provide links to existing street to the west and east of the site. The open spaces would also incorporate grassed areas, tree planting, landscaping and three park benches. The layout of the proposed houses is shown at Figure 1.



Figure 1: Proposed housing layout

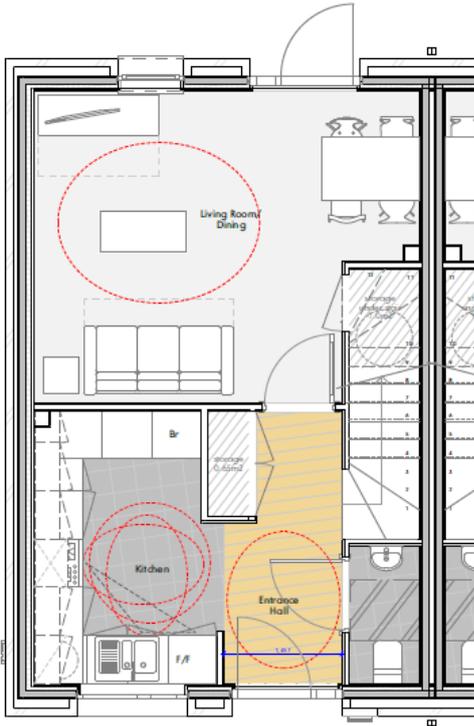
The houses would be presented in mix of house types, mainly involving red brickwork with a number incorporating grey brick. All the houses would have pitched roofs over 2-storeys, brick detailing and metal framed windows. Each house would incorporate a car parking space thereby achieving 100% parking provision across the development. Front and rear gardens of each house would have principal boundaries to the street defined on low walls and railings. A mix of low walls and timber fencing formed along boundaries adjacent to publicly accessible areas. Cycle and segregated bin storage would be provided with each garden. A visualisation of the proposed housing is shown at Figure 2. Typical elevations are shown at Figure 3 and typical layouts of the proposed houses are shown at Figures 4 and 5.



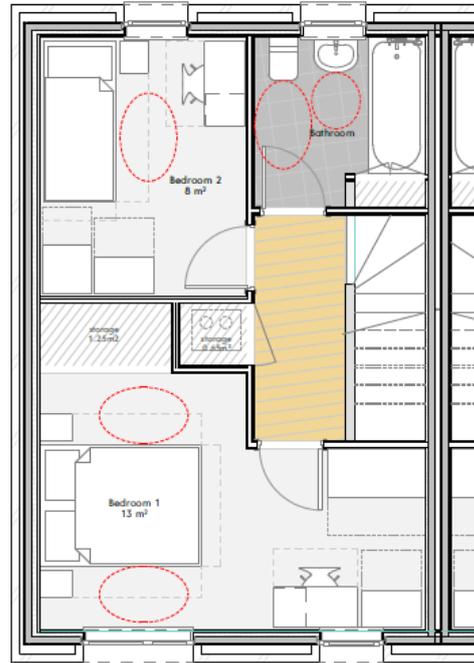
Figure 2: A visualisation of the proposed housing



Figure 3: Typical elevations

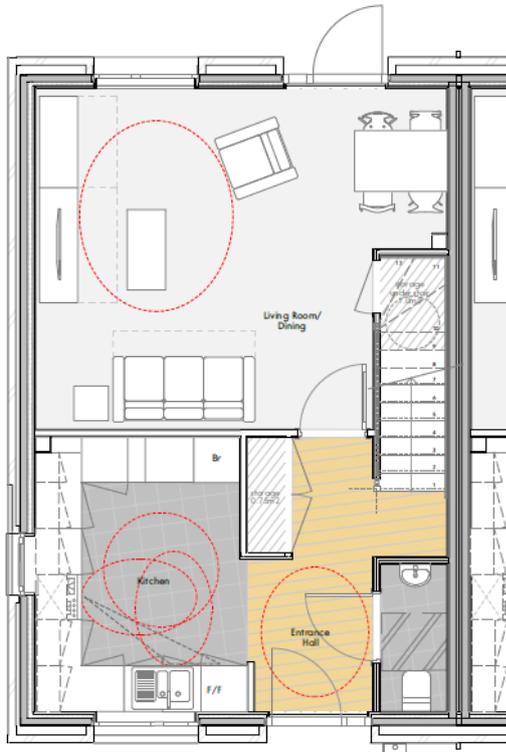


GROUND FLOOR

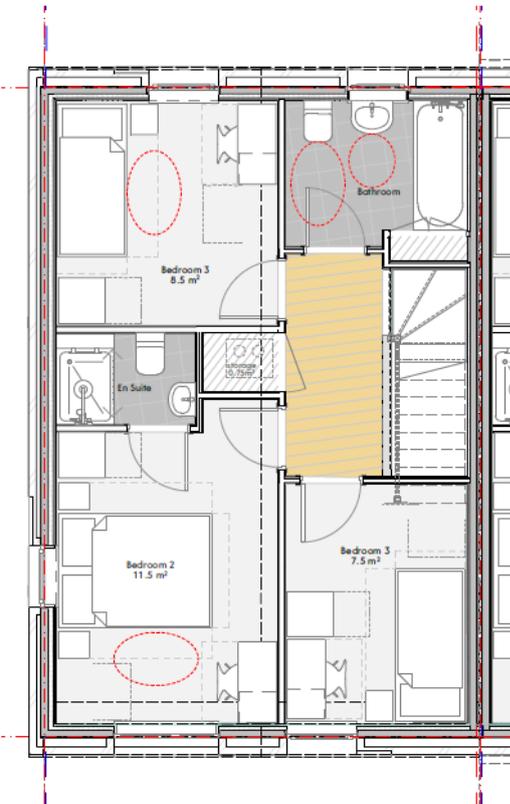


FIRST FLOOR

Figure 4: Typical layout for 2 bedroom house



GROUND FLOOR



FIRST FLOOR

Figure 5: Typical layout for 3 bedroom, 4 person house

Consultations

Local objections - A number of local objections have been received comprising 1 letter, 1 email and petition signed by 17 individuals. A local objector has submitted copies of 73 pre-application consultation responses undertaken by One Manchester. The responses to the planning application and pre-application consultation raise the following concerns:

- i. The objection predominantly relate to the loss of open green space and the resulting reduction of opportunities for children to play safely. It is also considered that the loss of this space would adversely affect the appearance and character of the area and be harmful to visual amenity. The existing open space is regarded as being important to the well-being of residents and supporting information has been submitted showing the benefits of retaining such spaces on physical and mental health. The space presents opportunities for both play and exercise, which is regarded as important in combating obesity. The loss of the open space would also harm local bio-diversity. The development would thereby undermine the quality of the local living environment;
- ii. Alternative sites should be considered for housing with a lesser impact on residential amenity and the quality of the local environment;
- iii. Investment should be made in the local area through refurbishment of the existing housing stock rather than building new houses;
- iv. The development would result in additional on-street car parking and traffic generation, which would be detrimental to pedestrian and highway safety;
- v. Residents have indicated that the area currently experiences a degree of crime and anti-social behaviour and maintain that the development would exacerbate these concerns through the loss of recreational space. It is also considered that additional on-street car parking would increase the localised risk of car crime.

The increase in residents will put pressure on local services and make it difficult to make appointments with GPs and secure school places.

The comments made at the pre-application were assessed by the applicant and informed the design and layout of the proposed development.

Highway Services – The following comments have been received:

- i. The addition of 23 houses is unlikely to generate any significant highway impact on the network. The submitted transport statement estimates an additional 17 trips during the AM/PM peak hours (between 8.00 am - 9.00 am and 5.00 pm – 6.00 pm) which can be absorbed on the existing highway network without undue capacity or safety concerns. Accident data has been reviewed for the site over the most recent five year period which demonstrates that there are no significant issues regarding highway design or infrastructure;
- ii. The site is located in a sustainable location within East Manchester and there are:

- a. Several bus services routed along Alan Turing Way to the west and Ashton Old Road to the south with connections across the area;
- b. Rail services are provided at Ashburys Station, which is in a walkable half mile radius of the application site;
- ii. The proposed layout includes 100% in-curtilage car parking for each dwellinghouse. All driveways would achieve dimensions of 3 metres x 6 metres and off site highways works should meet City Council standards. Each driveway should incorporate electric vehicle charge points to commit to ongoing sustainability;
- iii. The proposals would incorporate secure and weatherproofed cycle parking with the re should be made available within the curtilage of each property;
- iv. There are various routes, both sections of highway and footpaths acting as public rights of way, which currently dissect the site and therefore stopping up orders will be required to remove these as public rights of way. If the new access road from Herne Street is to be adopted it will need to meet City Council standards. The new highway layout should also accommodate a 20mph speed limit order similar to surrounding roads and should be included within the existing or proposed Stadium Controlled Parking Zone;
- v. All boundary treatments with frontage to the adopted highway must retain visual permeability from a height of 600mm upwards to maintain sight lines. All gating should open inwardly to prevent obstruction to footway users;
- vi. It is proposed that residential waste collection will take place from the kerbside, with residents responsible for moving the bin to the kerbside on collection day and back to the garden storage area thereafter. This is accepted in principle. Highways accept that a turning head will be not provided as this is likely to generate inconsiderate parking, as is currently experienced. There are a limited number of properties to be serviced from this location and as such the proposed collection strategy is accepted on this occasion;
- vii. The development should be related to a construction management plan to be implemented upon commencement of works.

Environmental Health – The following comments have been received:

- i. A construction management plan is requested;
- ii. The details of the submitted noise report should be amended to take account of potential noise sources from local noise sources from local commercial/industrial premises. The applicant has provided additional details that have been referred to Environmental Health and further comments will be reported;
- iii. The development should be undertaken in accordance with the submitted waste management arrangements;
- iv. The applicant has submitted an air quality assessment, which is considered to be acceptable. However, it is recommended that the development is undertaken in accordance with good practice principles. It is also recommended that electric vehicle (EV) charging points are provided.
- v. The development should be related to a condition to address historic land contamination.

Neighbourhood Team Leader (Arboriculture) – Any received comments will be reported to committee.

Flood Risk Management Team – The following conditions have been requested in relation to the following:

- i. No development shall take place until a scheme detailing surface water drainage works has been submitted. It should be designed in accordance with the technical standards for sustainable drainage;
- ii. If there is no clear adoption policy in place to take over the proposed drainage system after construction, a scheme should be provided detailing the arrangement for the implementation, maintenance and management of the sustainable drainage scheme.

Greater Manchester Police Design for Security - It is recommended that the physical security specifications set out in the submitted Crime Impact Statement should be added, if the application is to be approved.

United Utilities Water PLC (UU) – The applicant has been advised of the comments of UU with regard to surface water and foul water drainage as part of the development.

Environment Agency (EA) – Have requested additional information in relation to flood risk, which has been provided by the applicants. Additional comments are awaited from the EA in relation to this further information.

Greater Manchester Ecology Unit – The findings of the submitted ecology report are accepted. The proposed landscaping scheme is considered to be acceptable in relation to the inclusion of native species, wildflower grassland and an increase in overall tree numbers. The ecological report identifies opportunities for biodiversity enhancement in the form of bat and bird boxes and a condition is recommended to ensure to ensure the inclusion of these arrangements to enhance the biodiversity as part of the development.

Coal Authority – Have been consulted in response to the history of coal mining in the area and any comments will be reported.

Issues

National Planning Policy Framework (NPPF) - This Framework came into effect on 27th March 2012 and was amended and updated in February 2019. It sets out the Government's planning policies for England and how these are expected to be applied. It defines the Government's requirements for the planning system 'only to the extent that it is relevant, proportionate and necessary to do so'. It provides a mechanism through 'which local people and their accountable councils can produce their own distinctive local and neighbourhood plans, which reflect the needs and priorities of their communities'.

The Framework re-iterates that planning law requires that applications for planning permission must be determined in accordance with the development plan unless

material considerations indicate otherwise. The statutory status of the development plan remains as the starting point for decision making. However, paragraph 10 states that 'at the heart of the Framework is a presumption in favour of sustainable development' and, in 'decision-taking', this means that development proposals should accord with the development plan should be approved without delay unless: any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole or specific policies in this Framework indicate development should be restricted.

The Framework has been related to the proposed development, with particular emphasis given to the following:

- i. Chapter 5 Delivering a sufficient supply of homes - States that to support the Government's objective of significantly boosting the supply of homes, it is important that: a sufficient amount and variety of land can come forward where it is needed; the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay (paragraph 59). This should be reflected in the consideration of the size, type and tenure of housing needed for different groups in the community (paragraph 61). Chapter 5 states that the provision of affordable housing should not be sought for residential developments that are not major developments and that the re-use of brownfield sites should be encouraged (paragraph 63). It is considered that small and medium sized sites can make an important contribution to meeting the housing requirement of an area as they are often built-out relatively quickly (paragraph 68). It further requires that local planning authorities should support the development of entry-level exception sites, suitable for first time buyers (paragraph 71). In this case, it is considered that the size and tenure of the proposed development would positively contribute to the quality and supply of housing in the local area, secure the development of a brownfield site and thereby responds to an identified local housing need for family housing. It would also help to diversify the local housing market through the provision of high quality design that is capable of achieving sustainable future occupation. Chapter 5 would thereby be complied with.
- ii. Chapter 8: Promoting healthy and safe communities - States that planning decisions should aim to achieve healthy, inclusive, accessible and safe places, where crime and disorder (and the fear of crime) do not undermine the quality of life or community cohesion (paragraph 91). These considerations have been related to the layout and design of the proposed development and the incorporation of measures to address issues of security and potential opportunities for criminal and anti-social behaviour. Chapter 8 also requires consideration of the loss of open space and requires appropriate justification that such land is surplus to requirements. In this case, the land has been previously developed as housing has been grassed and maintained thereby giving amenity value to the area. As such the land has been previously been identified as open recreational space as part of the City Council open space audit. The applicant has provided an open space assessment that describes positive enhancement of the space to be retained and the proximity of Openshaw Park and Phillips Park, which are within 10 and 15 minutes

walking distance of the application site. Furthermore, the existing play area, adjacent to the site, would be retained and maintained in management of the applicant. On balance it is considered that the proposed loss of 0.5 hectares of the overall open space would be justified on the basis of the benefits of the development, the improvement to local space and the availability of parkland with the locality. The development would thereby accord with Chapter 8.

- iii. Chapter 9: Promoting sustainable transport - States that in assessing specific applications for development, it should be ensured that:
 - a) Appropriate opportunities to promote sustainable transport modes can be - or have been - taken up, given the type of development and its location;
 - b) Safe and suitable access to the site can be achieved for all users; and
 - c) Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree (paragraph 108).

It is considered that, given the magnitude of the development, the generation of traffic and vehicular movement would be predictable and capable of being accommodated within the local highway infrastructure. Satisfactory off-street car parking would also be achieved with opportunities for in curtilage cycle storage to promote more sustainable transportation options. The proposed development would thereby be consistent with the requirement of Chapter 9.

- iv. Chapter 11: Making effective use of land - States that planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions. It also encourages the use of previously developed or 'brownfield' sites (paragraph 117). Paragraph 122 states, amongst other things, that planning decisions should support development that makes efficient use of land, taking into account:
 - a) The identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it;
 - b) The importance of securing well-designed, attractive and healthy places.

It is considered that chapter 11 has been positively responded to as it would secure the re-use of previously developed land and secures housing with a high quality of urban design. It is also considered that the development would be appropriately related to the scale, proportions and character of development in the surrounding area.

- v. Chapter 12: Achieving well-designed places - States that good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities (paragraph 124). Paragraph 127 further states that planning decisions should ensure that developments:
 - a. Will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;

- b. Are visually attractive as a result of good architecture, layout and landscaping;
- c. Are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d. Establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials;
- e. Optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development.

The proposed development achieves a layout that relates appropriately to the surrounding context, including its acknowledgement of the character neighbouring houses. This includes the formation of front and rear gardens presented in a configuration that emphasises space between the proposed houses that maintains a sense of spaciousness throughout the overall site. The arrangement of the proposed houses and related landscaping would provide a setting the retained open space that would be beneficial to the environmental quality of the area. Given the quality of the proposed design, it is considered that the development would contribute to place making and the character of the local area thereby according with chapter 12.

- vi. Chapter 14: Meeting the challenge of climate change, flooding and coastal change - States that new development should be planned for in ways that:
 - a. Avoid increased vulnerability to the range of impacts arising from climate change;
 - b. Can help to reduce greenhouse gas emissions, such as through its location, orientation and design (paragraph 150).

It further states that all plans should apply a sequential, risk-based approach to the location of development - taking into account the current and future impacts of climate change to avoid where possible, flood risk to people and property (paragraph 157).

It is considered that the site responds positively to chapter 14, through the incorporation of design techniques that reduce the potential for carbon emissions and measures to managed surface water drainage and capable of being finalised through conditions. Similarly, the development has been support with a flood risk assessment that has been submitted with additional details. Although the finalised details are to be fully considered by the EA, there is nothing to suggest that these risk of flood presented by the development would affect its deliverability. The design of the proposed houses has been related to and environmental assessment, which indicated that the proposed house would have highly details designed capable of being delivering a strong environmental performance. The proposed houses would incorporate garden areas that would add surface water drainage, as would retained open space. Opportunities for landscaping and tree planting would also add to the reduction of carbon emissions.

Planning Practice Guidance (PPG) - On 6 March 2014 the Department for Communities and Local Government (DCLG) launched this planning practice guidance web-based resource. The PPG seeks to both simplify and clarify planning guidance easier and simpler. It is intended to be read in conjunction with the National Planning Policy Framework (NPPF) and is relevant to key planning issues of

significance to applicants and local authorities. In the following assessment of the proposed development has been given to the following aspects of the PPG:

- i. Consultation and pre-decision matters - The NPPG reasserts that local planning authorities are required to undertake a formal period of public consultation, prior to deciding a planning application. All necessary local consultation has been undertaken along the posting of site notices and press advertisement;
- ii. Design - Good quality design is considered to be an integral part of sustainable development. Achieving good design is about creating places, buildings, or spaces that work well for everyone, look good, last well, and will adapt to the needs of future generations. It is considered that the development present a contemporary housing design that is appropriately related to the scale and proportions of neighbouring houses;
- iii. Flood Risk Planning and Flood Risk - The proposed development has been assessed to determine if it represents a flood risk. For the purposes of applying the National Planning Policy Framework, 'flood risk' is a combination of the probability and the potential consequences of flooding from all sources. The application has been accompanied by a flood risk assessment and it is considered that appropriate on site drainage can be achieved;
- iv. Health and well-being - States those local planning authorities should ensure that health and wellbeing, and health infrastructure are considered in planning decision making. It also recognises that development can provide opportunities for healthy lifestyles through the promotion of and access to high quality open spaces and opportunities for play, sport and recreation. In this case the development would provide a high quality of design that would positively benefit the appearance of the area. It would also secure environmental improvement to the retained opens space, which would be subsequently maintained through a management condition;
- v. Air quality - The PPG states the relevance of air quality to a planning decision will be dependent upon the proposed development and its location. Consideration should be given to the likelihood that the development would:
 - a. Significantly affect traffic in the immediate vicinity of the proposed development site or further afield;
 - b. Introduce new point sources of air pollution;
 - c. Expose people to existing sources of air pollutants. This could be by building new homes, workplaces or other development in places with poor air quality;
 - d. Give rise to potentially unacceptable impact (such as dust) during construction for nearby sensitive locations;
 - e. Affect biodiversity.

It states that where necessary, mitigation measures should be specifically related to the location of the development and be proportionate to the likely impact. An air quality assessment has been provided, which demonstrates that the development would not unduly affect existing air quality following occupation of the houses. The

inclusion to electric vehicle charging point will also help to management future vehicle emissions.

- v. Land affected by contamination - States that the contaminated land regime under Part 2A of the Environmental Protection Act 1990 provides a risk based approach to the identification and remediation of land where contamination poses an unacceptable risk to human health or the environment. It also states that only a specific investigation can establish whether there is contamination at a particular site, but the possibility should always be considered particularly when the development proposed involves a sensitive use such as, in this case, house with gardens. The application has been accompanied with a ground condition survey and it is considered that identified land contamination issues will be addressed through details required by condition.
- vi. Noise - Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. The PPG sets out the potential approaches to responding to noise and appropriate mitigation, which have been applied in the consideration of the proposed development. It is considered that identified noise impacts can be attenuated through the detailed design of the development.
- vii. Travel plans and traffic / transport assessments - The PPG has been considered in respect of the traffic and highways issues presented by the development and potential measures to secure a reduction in reliance on private car usage. It is considered that the local highway network is capable of accommodating the additional traffic generated by the development.

Manchester's Local Development Framework: Core Strategy - The Core Strategy Development Plan Document 2012 -2027 ('the Core Strategy') was adopted by the Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long term strategic planning policies for Manchester's future development. A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents.'

The following policies are relevant to the proposed development:

Policy SP 1 (Spatial Principles)

Policy SP1 specifies the Core Development Principles for parts of the City. In this case the relevant principles relate to the extent to which the development:

- a. Makes a positive contribution to neighbourhoods of choice including the creation of well designed places that enhance or create character; making a positive contribution to the health, safety and well-being of residents, considering the needs of all members of the community regardless of age,

- gender, disability, sexuality, religion, culture, ethnicity or income and to protect and enhance the built and natural environment;
- b. Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible;
 - c. Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.

It is considered that the proposed development would contribute positively to the formation of a sustainable community, which would be beneficial to the quality of the living environment experienced by existing and future residents. This would be achieved through the development of a high quality and sustainable design that would respond to the on-going need for housing in the local area and add vibrancy to its character. Policy SP1 would therefore be positively responded to.

Policy EN1 (Design Principles and Strategic Character Areas)

Policy EN 1 states that opportunities for good design to enhance the overall image of the City should be fully realised through the implementation detailed design principles that reinforce and enhance the local character of that part of the City and supports the achievement of the Core Strategy Strategic Objectives. In this case, it is that the proposed design would achieve a high quality that would positively contribute to the character of the area and improve the standard of residential accommodation in the locality. The demarcation of public and private space formation of garden areas and hard surfacing and boundary treatments would secure a satisfactory relationship to the streetscene. The development would involve a contemporary interpretation of the characteristics surrounding area, through the inclusion of gable elevation between pitched roofs and use of contrasting shades of brickwork. The design would be lifted through the inclusion of metal framed windows and arrangement of brick courses. The development would thereby comply with policy EN1.

Policy EN4 (Reducing CO2 Emissions by Enabling Low and Zero Carbon Development)

Policy EN4 has been related to the assessment of the submitted statement detailing measures to reduce CO2 emissions and to secure energy efficiency. It is considered a high quality of energy efficient design, which would provide the most effective means of reducing carbon emissions given the physical constraints of the site and magnitude of development. On this basis, policy EN4 would therefore be appropriately responded to.

Policy EN 8 (Adaptation to Climate Change)

Policy EN8 states that all new development will be expected to be adaptable to climate change in terms of the design, layout, siting and function of both buildings and associated external spaces. In this case of this application reference has been given to the adaptability of the development to climate change with particular reference to:

- i. Minimisation of flood risk by appropriate siting, drainage, and treatment of surface areas to ensure rain water permeability;
- ii. The need to control overheating of buildings through passive design;
- iii. The opportunity to provide linked and diverse green space to enhance natural habitats, which will assist species adaptation.

Policy EN8 has been related to the proposed sustainable design of the proposed houses and arrangements to secure appropriate drainage management. The development would incorporate individual garden areas along with retained public open spaces that would further enhance drainage in and around the application site. It is considered that policy EN8 would be responded to satisfactorily.

Policy EN 9 (Green Infrastructure)

Policy EN 9 states that new development will be expected to maintain existing green infrastructure in terms of its quantity, quality and multiple function. Policy EN9 is relevant to the proposal as it involves the development of previously developed open land. In this case, the development would allow the retention of an area of open space with environmental improvements that would benefit the local community, Trees removed by the development will be replaced through a landscaping scheme. Rear gardens would be presented to the open space thereby increasing the sense of spaciousness across the development. On balance, it is considered that development would be appropriately related to policy EN9.

Policy EN 14 (Flood Risk)

Policy EN 14 states that in line with the risk-based sequential approach, development should be directed away from sites at the greatest risk of flooding and towards sites with little or no risk of flooding. The applicant has provided a flood risk assessment that requires supplementary details relating to surface water attenuation and drainage management. The applicant has agreed to a condition that would secure the submission of appropriate sustainable drainage details to be implemented as part of the development. Further details contained within a revised FRA have been referred to the Environment Agency for further comments. It is considered that issues relating to flood risk and drainage are capable of being addressed through detail design to secure compliance with policy EN14.

Policy EN15 (Biodiversity and Geological Conservation)

Policy EN15 states, amongst other things, that the developers will be expected to identify and implement reasonable opportunities to enhance, restore or create new biodiversity, either on-site or adjacent to the site, contributing to linkages between valuable or potentially valuable habitat areas where appropriate. Policy EN15 has been related to the assessment of the ecological evaluation of the site and the details of the ecology report. A condition has been included to ensure the implementation of the development in accordance with the ecology report, which includes recommendations for the provision of bat and bird boxes and native tree and plant provision that would benefit wildlife. Policy EN15 would therefore be complied with.

Policy EN 16 (Air Quality)

An air quality assessment has been submitted and assessed. It is accepted that impacts on air quality would be mainly related to the construction stage and that impacts during the operational stage would be limited to residential traffic movement. Given the magnitude of the development and the limited frequency of vehicle movement, it is not anticipated that there would be a significant impact on local air quality. A pre-occupation condition requiring the provision of an electric vehicle charging points for each bungalow has been agreed. It is considered that these measures would help to maintain and / or improve future localised air quality pursuant to policy EN16.

Policy EN18 (Contaminated Land and Ground Stability)

Policy EN18 states that any proposal for development of contaminated land must be accompanied by a health risk assessment. This application has been accompanied by a Desk Study and Geo-Environmental Assessment, which has been assessed by the Contaminated Land Section whose recommendations have indicated that identified issues are capable of resolution by the attachment of a contaminated land condition to secure compliance with policy EN18.

Policy EN19 (Waste)

Policy EN19 requires consideration of the submitted details relating to determine if the applicant has satisfactorily demonstrated how:

- i. Both construction and demolition waste will be minimised and recycled on site wherever possible;
- ii. The sustainable waste management needs of the end user will be met.

Policy EN19 has been related to the proposed waste management arrangements, which are considered to be acceptable in terms of capacity, with issues relating to the transfer of waste containers to collection points addressed within this report.

Policy H1 (Overall Housing Provision)

Policy H1 relates to the City Council strategy for the delivery of new housing between March 2009 and March 2027. The proposed development responds positively to policy H1 by:

- i. Contributing to the creation of mixed communities by providing house types to meet the needs of a diverse and growing Manchester population, including elderly people, disabled people and people with specific support requirements;
- ii. Supporting growth on previously developed sites in sustainable locations and which takes into account the availability of developable sites in these areas;
- iii. Ensuring that the design and density of the a scheme contributes to the character of the local area;

- iv. Making appropriate provision for parking cars and appropriate levels of sound insulation;
- v. Being designed to give privacy to both its residents and neighbours.

Policy H 4 East Manchester

The proposed development would accord with Policy H4 and its aspiration for the delivery of new housing in East Manchester and the delivery of high quality and sustainable family housing.

Policy H 8 Affordable Housing

Policy H8 sets the requirements for affordable housing or an equivalent financial contribution. It is relevant in this case as development would exceed the 15 or more threshold for affordable housing provision. Policy H8 requires that developers use a 20 % target for the incorporation of affordable housing. In this case, the developer has indicated that the development would deliver 17 social rented and 6 shared ownership houses under Homes England 2016-21 grant funded programme. In order to ensure a minimum provision of 20% affordable rented accommodation in perpetuity, including circumstances where the applicant (One Manchester) transfers ownership of the land and retained rented houses to another party, it is recommended that the development be related to a s106 Agreement. This agreement would ensure a minimum 20% contribution towards the provision of affordable housing in perpetuity. On the basis policy H8 would be accorded with.

Policy H11 (Houses in Multiple Occupation)

Policy H11 has been related to a condition to ensure that the proposed housing is retained as Class C3 accommodation. This accords with the requirement of policy H11, of securing a sustainable supply of family housing and maintaining an appropriate mix housing type and tenure.

Policy T1 (Sustainable transport)

Policy T1 relates to the delivery of sustainable, high quality, integrated transport system, which encourages a modal shift away from car travel to public transport, cycling and walking and prepare for carbon free modes of transport. In this case that development incorporates sufficient in curtilage car parking, with sufficient space within rear gardens to provide secure cycle storage shed to meet residents' requirements. On this basis, it is considered that the development responds positively to policy T1.

Policy T2 (Accessible areas of opportunity and need)

Policy T2 states that the Council will actively manage the pattern of development to ensure that new development: is located to ensure good access to the City's main economic drivers, including the regional centre and to ensure good national and international connections; is easily accessible by walking, cycling and public transport; connecting residents to jobs, centres, health, leisure, open space and educational opportunities. The sustainable location of the application site, in terms

of access to public transport and proximity to services within local centres would ensure that the development would be suitably related to policy T2.

Policy DM1 (Development Management)

Policy DM1 states that all development should have regard to the following specific issues for which more detailed guidance may be given within a supplementary planning document. Relevant considerations in this case are:

- a. Appropriate siting, layout, scale, form, massing, materials and detail;
- b. Impact on the surrounding areas in terms of the design, scale and appearance of the proposed development to ensure that development has regard to the character of the surrounding area;
- c. Effects on amenity, including privacy, light, noise, vibration, air quality and road safety and traffic generation;
- d. Accessibility: buildings and neighbourhoods should be fully accessible to disabled people with new development providing access to all via sustainable transport modes;
- e. Community safety and crime prevention;
- f. Design for health;
- g. Adequacy of internal accommodation and external amenity space;
- h. Refuse storage and collection.

Policy DM1 points a - h (inclusive) have been related to the assessment of the proposals with regard to its potential impact on residential amenity and the contextual relationship of the new housing to the local built environment. It is considered that, for the reasons set out in this report, the above points have been responded to satisfactorily.

Saved Unitary Development Plan (Saved UDP) Policies -The following saved Unitary Development Plan policy is also considered to be relevant:

Policy DC7 (New Housing Development)

Policy DC7.1 states that the Council will negotiate with developers to ensure that new housing is accessible at ground floor level to disabled people, including those who use wheelchairs, wherever this is practicable. Policy DC7 also requires that a satisfactory quality of development is achieved. In this case the layout of the proposed houses and the quality of design would accord with residential quality guidance. It is also considered that satisfactory access for people with disabilities would be achieved. Policy DC7.1 would therefore need to be complied with.

Policy DC26 (Development and noise)

The following elements of policy DC26 are considered to be relevant:

Policy DC26.1 has been related to the proposals contribution to the local noise environment and how existing noise sources, including noise from neighbouring industrial / commercial uses, may impact on the proposed housing.

Policy DC26.4 requires that where an existing noise source might result in an adverse impact upon a proposed new development, or where a new proposal might generate potentially unacceptable levels of noise, consideration is given to measures to deal with it satisfactorily. This particularly relevant given the proximity of the site to established industrial uses.

Policy DC26.5 has been related to the assessment of the development, in terms of measures to minimise the impact of noise on future residents, including the incorporation of noise insulation.

The site is relatively close to the Etihad and Academy Stadiums, which produce noise during events, which is predictable but this noise not generally reflective of the predominant noise environment, which is predominantly residential and affected by noise from major roads. It is considered that issues relating to noise attenuation can be addressed through the recommended condition and that policy DC26 is capable of being accorded with.

Guide to Development in Manchester: Supplementary Planning Document and Planning Guidance - The Guide aims to support and enhance the on-going shaping of the City by providing a set of reasoned principles which will guide developers, designers and residents to the sort of development we all want to see in Manchester.

The following paragraphs are of particular relevance:

- i. Section 2 Design - Discusses the importance of the design of new development in relation to surrounding neighbourhoods and the character of its streets, in terms of its layout, design, scale, massing and orientation of its buildings to achieve a unified urban form to enliven the neighbourhood and its sustainability. The density of the development should also be informed by the characteristics of an area and the specific circumstances of the proposals. It is considered that these design principles have been incorporated into the development.
- ii. Section 3 Accessibility - The development achieves a satisfactory arrangement for inclusive access to the proposed houses, including level access to external door thresholds and internal circulation space.
- iii. Section 4 Environmental Standards - Identifies the need to ensure that development is environmental sustainable and designed to reduce carbon emissions. It also identifies the need to ensure the provision of appropriate waste management is incorporated into the development together with arrangements for construction management. These issues have been positively responded to through the submitted planning application, which would achieve a high quality of environmentally sustainable development.
- iv. Section 7 Housing Density and Mix - States that the composition of the residential development has been assessed to determine if the development positively contributes to the range of housing choices and assists the sustainability of these neighbourhoods. It is considered that the development responds positively to these objectives.

- v. Section 8 Community Safety and Crime Prevention - Relates the importance of creating safe environments through the incorporation of informal surveillance and crime prevention measures as an integral part of new development. These objectives have been related to the assessment of the proposed design and layout of the development.

Providing for Housing Choice Supplementary Planning Document (SPD) and Planning Guidance (adopted 2nd September 2008) - This document provides planning guidance about the mix of new housing provision required in Manchester to meet the requirements of the City's planning policies and government guidance about planning policies for housing provision, The City Council is committed to establishing a strategy for affordable housing provision in Manchester. It is important that everyone living in Manchester has the opportunity of a decent, affordable and accessible home and that the range of available housing both supports the City's economic growth and develops and sustains neighbourhoods, attracting families and workers.

The proposed development would be exceed the 15 unit threshold for the provision of affordable housing. The development would be delivered through a social housing provider with a stated commitment to progressing new proposals in Openshaw for social rent, affordable rent, shared ownership and rent to buy tenures. The development would deliver 17 social rented and 6 shared ownership and arrangements will be related to the development to ensure the provision of 20% affordable housing provision within the development in perpetuity. It is therefore considered that the proposal would be positively contribute to the objectives of responding to identified housing need the provision of varied housing types and tenures.

Manchester Residential Quality Guidance (MRQG) - Sets out the direction for the delivery of sustainable neighbourhoods of choice where people will want to live and also raise the quality of life across Manchester. It was approved by the Executive at its meeting on 14 December 2016. The delivery of high-quality, flexible housing will be fundamental to ensuring the sustainable growth of Manchester. To achieve the City's target of carbon neutrality by 2050, residential schemes will also need to be forward thinking in terms of incorporating the most appropriate and up to date technologies to significantly reduce emissions. It is therefore essential for applicants to consider and integrate the design principles contained within the guidance into all aspects of emerging residential schemes. In this respect, the guidance is relevant to all stages of the development process, including funding negotiations, the planning process, construction and through to operational management. It is considered that the proposed housing layout would achieve appropriate standards of internal space and a quality of design that would contribute to the character of the local area and thereby would encourage sustained long term occupation. MRQG would thereby be complied with.

Manchester's Great Outdoors – A Green and Blue Infrastructure Strategy for Manchester (GI) (2015 -25) adopted July 2015 – The strategy sets out a framework to guide the maintenance of and access to green space and blue space, i.e. waterways, canals and rivers etc., within the city. The strategy builds on the

investment to date in the city's green infrastructure (GI) and the understanding of its importance in helping to create a successful city. It states that by 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. The city's communities will be living healthy, fulfilled lives, enjoying access to parks and greenspaces and safe green routes for walking, cycling and exercise throughout the city. Although the development would result in a partial loss of the existing open space, it would deliver high quality new housing with opportunities for, what the GI refers to as, 'smaller scale GI interventions', such as street planting, community accessible green space and private gardens, which would increase the attractiveness of the area and its resilience to climate change. The strategy recognised the need to develop brownfield sites and provide high quality and multifunction GI in East Manchester. In this case, the development would respond to these objectives by ensure in the retained green space would be linked to the formal play area adjacent its southern boundaries. The application site also benefits from access to high quality parks in reasonable walking distance. The development would also improve landscaping, tree planting and the provision of permeable routes through appropriately managed space. The introduction of new housing in the area help to create neighbourhoods of choice. The incorporation of private garden areas would also help to deliver the small scale GI intervention envisaged by the strategy, particularly with regard to climate change and biodiversity. On balance it is considered that the development would appropriately related to the strategy.

Positive and proactive engagement with the applicant - An amendment to the DMO, which came into effect on 1st December 2012, requires every decision notice relating to planning permission and reserved matters application to include an explanation as to how the local planning authority have worked with the applicant in a positive and proactive manner based on seeking solutions to problems which arise during the determination of the planning application.

In this case, officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. The applicant's agent engaged in pre-application discussions with officers. On-going dialogue throughout the consideration of the planning application has secured sufficient and satisfactory information that has allowed the planning application to be fully assessed. On balance, it is considered that the development is of a scale and magnitude that would relate positively to the surrounding area, be capable of achieving a high standard of urban design and add to the provision of affordable housing in the city.

Principle of the development – Whilst acknowledging that the application site provides a local green space, the site has been previously developed and can therefore be considered as a brownfield site within an urban context. The application has also been identified in the 2019 Strategic Housing Land Availability Assessment (SHLAA) as a capacity site. The Council has therefore identified the site as having potential for future new housing development subject to assessment of its suitability through a planning application. As such the site presents a development opportunity, which would contribute positively towards City Council house building targets and the provision of affordable, social housing and diversity of tenure. The development would result in the loss of a significant portion of existing open space. However, a retained section of central space would be maintained within a more useable and

coherent arrangement of footpaths, which would link existing and proposed housing and improve access to retained formal outdoor play facilities. The density of the development and the inclusion of front and rear garden areas would maintain a sense of spaciousness across the site and acknowledged the character of housing in the locality. It is therefore considered that the proposals are capable of delivering a high quality and sustainable development, which would positively contribute to the quality and appearance of the local area and its regeneration. It is also considered that the amenity of existing and future residents would be safeguarded through the detailed design of the proposals and the recommended condition that would ensure the appropriate delivery of the development.

Affordable housing – The applicant has submitted an affordable housing statement that confirms the following:

- i. The development would deliver 17 social rented and 6 shared ownership houses under Homes England 2016-21 grant funded programme and meet the related funding conditions for social rent. The development would aim to diversify the affordable homeownership offer and attract new residents into an edge of city location. Residents would have the opportunity to purchase an equity share in the property with an option to ‘staircase’ or increase equity shares in future years.
- ii. The development would form part of the applicant’s programmed delivery of social rent, affordable rent, shared ownership and rent to buy tenures in Openshaw.

The applicant would deliver the proposed development and manage the retained social rented and external spaces. Notwithstanding the applicants’ stated objective of delivering affordable housing, it is considered that safeguards need to be put in place to ensure that a minimum of 20% of the proposed housing is maintained as affordable in the future, i.e., to ensure that appropriate social rents are maintained should the proposed houses be transferred to another landlord and / or the houses are made available for home ownership. To achieve this, it is recommended that a s106 Agreement be related to the development, which:

- i. Identifies a total of 5 houses as a mix of affordable housing contribution to be delivered in perpetuity within the development site;
- ii. This contribution would be related to specific plots with a mix of house types within the development site and would require their maintenance for social rent.

Should members be minded to approve the development, it is recommended that a s106 Agreement be included with the decision to ensure that delivery of the above objectives.

Reduction in open space - The application site was identified as amenity green space within the Council’s 2009 assessment of open space. The applicant has therefore undertaken an assessment of the potential impact of the partial loss of this space as a result of the development. The applicant has also sought to address the concerns of neighbouring residents. The assessment states that, notwithstanding the

finding of the open space audit, the application site had previously been occupied by housing that was demolished as part of a phased demolition programme. The development therefore relates to a brownfield site that planning policy identifies as being suitable for residential development.

Notwithstanding the above, the development would result in the loss of 0.5 hectares of the open space. The main section of the retained space would be equivalent to the side of a 5-side football pitch and would therefore provide opportunities for play and recreation. The land also extends northwards and eastwards thereby providing opportunities for landscaping and tree planting.

The applicant has submitted a landscaping plan demonstrates a commitment to replacement tree planting. The development would result in the removal of 11 trees. However, a submitted landscaping scheme indicates that, in addition to 6 retained trees, 70 replacement trees of various species would be provided along with 12 fruit trees. This would in addition the planting of shrubs, wild flowers and climbing plants would also be undertaken. Avenues of trees would be formed on the east side of the proposed access road and adjacent to the footpath bisecting the site from west to east. The applicant also proposed the provision of asphalt surface to roads through the application leading to and across the open space.

New street lighting would be provided within the reconfigured car parking areas and adjacent to the proposed footpaths across the open space thereby supplementing the existing lighting provision. New park benches would also be provided within the open space. A condition has been included to ensure the approval of boundary treatments adjacent to publicly accessible areas, alleyway gates and street furniture. It is recommended that the street furniture details should include details of the proposed park bench to ensure it incorporates 'age friendly' design specifications to encourage its inclusive use and to provide opportunities for social interaction. The condition also includes the provision of measures that would facilitate access by wheelchair and pushchair but deter access by motorcyclists. It is considered that these measures would enhance of the appearance and quality of the open space. It would also improve the setting of the retained adjacent equipped play area.

The relationship of the site to existing open space provision, an assessment of open space has been submitted by the applicant. The assessment indicates that high quality parks are located within walking distance of the site, i.e., the well-equipped and maintained Openshaw Park is a 10 minute walk away and the listed Phillips Park is a 15 minute walk. The formal equipped play area to the south of the site (adjacent the Newtown Close) would be retained and would benefit from the environmental improvements to the adjacent open space.

It is considered that the loss of open space is, on balance, justified as:

- i. The development would positively contribute towards the much needed delivery of family housing;
- ii. The development would mitigate against the extent of the loss of open space through significant environmental improvements; the detail of which would be delivered through a condition;
- iii. The site benefits from access to high quality local parks.

On balance, it is considered that the development would lead to the formation of a managed open space, which would further contribute to the amenities of the area through its layout and improved connectivity to neighbouring streets for pedestrian and cyclists.

Future use of the development - A condition has been included for purposes of clarity and to confirm that the authorised development relates to the formation of 10 x two bedroom, 3 person houses and 13 x three bedroom, 4 person houses. A separate condition has been included to facilitate the appropriate future use and management of the authorised houses to limit their future occupation to Class C3 (a) not precluding occupation by two unrelated people sharing a property. This condition is considered necessary to secure the appropriate and sustainable occupation of the proposed houses.

Residential amenity - It is considered that the development would deliver high quality housing with an appropriate balance of built form and garden / amenity space. The proposed design would provide a contemporary elevational expression that acknowledges the pitched roof and hipped roof composition of neighbouring 2-storey houses. Satisfactory distances would be achieved between proposed and neighbouring habitable room windows. Distances of 21 metres would be achieved across streets and between existing and propose principal elevations. Minimum distances of between 10 and 13 metres would be achieved where secondary windows would face towards either existing or proposed blank gable elevations. Appropriate relationships to the streetscene would be achieved through the provision of appropriate boundary treatments that would define public and private space. The applicant has reviewed the height and design of boundary treatments potential affecting vehicular visibility and security adjacent to public spaces. Further potential noise impacts would be addressed through a condition requiring supplementary details to the submitted noise report. It is likely that any noise would related to traffic. Although the development would generate additional noise and activity, it would be characteristic of residential occupation. The house would front on to existing streets with 4 houses facing on to the new access road. It is therefore considered that this additional activity would be absorbed into the characteristic interaction and movement of residents in the locality. It is considered that satisfactory waste management arrangements are capable of being delivered. However, a condition has been recommended requiring further details of the appearance of bin stores within front garden areas. On balance, it is considered that the development would not unduly affect or harm residential amenity and would bring positive benefits to the built environment and quality of retained open space.

Permitted development restrictions - In order to maintain and safeguard residential amenity and in response to the constraints of the substantive site, it has been considered appropriate to restrict the permitted rights that would otherwise be available to householders. Conditions have therefore been recommended in the interests of residential amenity and to ensure that the following development is not undertaken without the permission of the City Council as local planning authority:

- i. The erection of extensions, porches and outbuildings;
- ii. The replacement of authorised boundary treatments;
- iii. The insertion of additional windows.

Residential space standards - The proposed development has been assessed against the criteria set out in the adopted Manchester Residential Quality Guidance. The design of the proposed house types would achieve the required internal floorspace relating to 2 bedroom, 3 person, 2 storey dwellings and 3 bedroom, 4 person, 2 storey dwellings, ie, 70 sq metres and 87 sq. metres respectively. The applicants have sought to respond positively to the constraints of the site. The proposal would also secure a development incorporating design characteristics that would also positively contribute to the quality of local housing, the streetscene and secure a reduction in carbon emissions. It would also represent a sustainable development, in terms of the location of the application site to public transportation links and proximity of local shops and services. The proposed development would thereby positively contribute to sustainable growth in Manchester and thereby respond positively to the Manchester Residential Quality Guidance.

Magnitude of development - It is considered that the development appropriately relates to the constraints of the application site, which is considered to have capacity for the proposed number of units. A condition has been recommended that relates the undertaking of the proposed development in accordance with specified number of units and ensures their future use for Class C3 purposes.

Siting – The majority of the proposed houses (19) would face towards existing streets with the remaining (4) houses presented to the new access road. It is therefore considered that the development would have a strong and cohesive relationship to the streetscene with opportunities for social interaction and natural surveillance. The formation of principal elevations would respond to the configuration of the site, secure appropriate car parking arrangements and defined public and private space. The retained open space within the centre of the site would provide an open setting for the proposed houses and would also benefit the amenities of existing neighbouring residents. This would be enhanced by the arrangement of rear garden areas to the main west part of the development site. Distances of 21 metres would be achieved existing and proposed houses fronting Beede Street, Mayton Street and Herne Street. A distances of 12 metres would be maintained between rear habitable room windows and the nearest blank elevations within the development, i.e. between block 4 and 9. Minimum distances of 13 metres would be maintained from the blank gable elevation of the nearest extended property at 43 Herne Street. To the east of the site, a distance of 10 metres would be maintained between the proposed side windows overlooking the footpath and the blank side elevation of the nearest house at 59 Herne Street. It is considered that the development would secure a configuration that would be appropriately related to the constraints of the site, neighbouring housing, retained open space and the streetscene.

Height, scale and massing - The surrounding area is characterised by 2-storey housing. The proposed development would respond positively to this context through the formation of 2-storey housing with maximum above ground level ridge heights of 8.75 metres and eaves heights of 5.69 metres applied across the development thereby achieving characteristic scale parameters across the site. The proportions of individual houses would be relieved through the incorporated elevational articulation, the design and sizing of windows, the use of materials and application of contrasting brickwork. The proportions of the development would be enhanced through the

formation of spaces between the 3 unit terraces and semi-detached houses. The development is therefore considered to be proportionate and satisfactorily related to the surrounding context.

Design - The incorporation of gable elevations and pitched roofs is an acknowledgement of the style and character of surrounding housing albeit at a reduced scale. Notwithstanding the above, the elevational composition represents a more contemporary design response, involving the composition of brickwork and metal framed windows and porch details. It is considered that the use of red brick with single unit of grey brick in each terrace would give the development visual interest and identity within the context of the streetscene. The elevational treatment would be animated with high quality metal framed windows incorporating contrasting brickwork detailing. Visual cohesion throughout the development would be achieved through the consistent ridge and eaves height. Similarly, the use of brick walls and railings would give the development identify with the streetscene and satisfactorily demarcated public and private space. A condition has been included to ensure the approval of external materials. The proposed design is considered to be acceptable.

Accessibility - The proposed development would achieve satisfactory access across level thresholds to front and rear external doors and a condition has been included to ensure that these arrangements are delivered as part of the development. The internal layout of the houses would be satisfactorily wide to external and internal doors and circulation spaces. The applicant has confirmed that the development has been designed to facilitate future adaptation to meet changing circumstances, including accessibility needs. Within the wider site, an accessible car parking bay would be provided within reconfigured car parking areas. The rearranged layout of footpaths within the site and across the open space would improve access for pedestrian, wheelchair / pushchair users and cyclists. It is therefore considered that satisfactory and inclusive access would be provided.

Crime and Security – A condition has been included to ensure that the development is implemented in accordance with the recommendations of the submitted crime impact survey. In addition, the applicant has agreed to gate the alleyways to be formed adjacent to the southern boundaries with existing houses at 31 – 34 Herne Street. The applicant has also reviewed the design of the proposed 2.1 metre high fencing to be erected to garden boundaries adjacent to public areas to avoid the formation of potential footholds in the lower section of brick walls. The development incorporates windows to side elevations to houses facing the street to aid natural surveillance. In curtilage car parking has been designed to be overlooked to reduce the risk of crime. The applicant has indicated the introduction of lighting within the retained open space to the benefit of public safety. On this basis it is considered the security issues have been positively responded to.

Boundary treatment - The proposed principal street boundaries would be defined by a mix of brick walls and railings. A combination of brick walls and fence panels would be formed to separate public areas and private gardens. The height of this fencing would rise to a height of 2.1 metres where enhanced security would be required. The design of this fencing has been suitably amended. The incorporation of alleyway gating within the southern section of the site is a positive addition. A condition has been recommended to address visibility issues relating to the car parking spaces to

the proposed northern house to Block 3 and the northern house Block 4. On balance, it is considered that the proposed boundary treatments would enhance the appearance of the development site, define public and private spaces and suitably enhance the security of the development.

Reducing carbon emissions and sustainable design - The applicant has submitted a comprehensive energy statement that indicates that the development would seek to achieve 'PassivHaus' accreditation, involving high levels of insulation and the formation of an air tight envelope, mechanical heat recovery ventilation and triple glazed windows. The design has been related to 'Be Lean, Be Clean, Be Green' principles that would achieve:

- i. Incorporate solar control treatment where possible on effected elevations;
- ii. Include high thermal performing fabrics beyond 2013 Part L (Building Regulations) limits;
- iii. Maximize daylight where possible;
- iv. Increased air-tightness of the building;
- v. Consider use of developing technologies where technically and economically feasible;
- vi. Ventilation - All areas would be ventilated to mitigate against overheating. Mechanical ventilation would be provided with heat recovery to minimise heating energy input. Variable speed fans will also be provided to limit fan power consumption;
- vii. Daylighting should be optimised to reduce reliance on artificial lighting and luminaires. Toilet, changing, store rooms etc. will be provided with presence detection. 100% Energy efficient lamps and LED lighting will be used throughout the development. A 'Showersave' system would be installed utilising water recovery.

Details have not been provided in respect of the inclusion of PV panels. A pre-occupation condition has been included to ensure approval of these details as part of the development should they need to be installed. The applicant envisages that, if the development achieves the high standard of environmental design required by Part L of the Building Regulations, PV panels may not be required. It is considered that the development can deliver houses with a high environmental performance and thereby achieve appropriate management of carbon emissions. The applicant has indicated that all properties will be wired to facilitate the future charging of electric vehicles. A condition has been included to ensure that these charging points are delivered as part of the development.

The development would be capable of incorporating the following measures to its sustainability, reduce carbon emissions and reduce its environmental impact:

- i. Cycle storage provision;
- ii. Tree planting and landscaping;
- iii. Sustainable drainage.

On this basis, it is considered that the development has been related to environmentally sustainable design principles.

Car parking and highways issues – The development would necessitate ‘stopping up’ orders to reconfigure footpaths and undertake the reconfiguration of car parking areas within the site. It is considered that these revised arrangements would improve communal car parking and secure permeable routes through and across the site with added natural surveillance. Although the number of footpaths would be reduced access through the site would be maintained. The development would be supported with 100% car parking with appropriately sized in-curtilage space provided within each garden area. The revised highways layout servicing houses and car parking areas to the south and east of the site would provide appropriate manoeuvring space. A condition has been recommended to ensure the provision of a fence with appropriate visibility at the vehicular access point to the car parking space to the northern house to Block 3 and the northern house to Block 4. Cycle storage has been provided within the rear garden of each house, which is considered to be satisfactory. The local highways networks would be capable of accommodate the traffic generated by the proposed 23 houses, which would be located in a previously developed site. It is not considered that the development would have an unduly harmful impact on pedestrian and highways safety subject to the resolution of the previously discussed boundary issue to one of the proposed houses. An off-site highways condition has been recommended to relating to the undertaking of necessary works to the highways adjacent to the site.

Stopping up existing footpaths – The development would necessitate the ‘stopping up’ or closure of a series of footpaths that intersect the existing open space, in order to facilitate the siting of the proposed houses. A replacement of suitably surfaced footpaths would be formed as part of the development. These footpaths would provide more direct and permeable routes through site and provide access to the retained open space, linkages to neighbouring streets and car parking bays. The reduction in footpaths would also allow the formation of 2 sections of grassed space that would be suitable for play and recreational use. It is therefore considered that the closure of existing footpaths would be acceptable and would be delivered through the Highways Act.

Off-site highways works – A condition has been included to ensure that all necessary works to the adjacent highways that are related to the development are undertaken prior to the occupation of the development. A separate condition has been included to ensure that the agreed arrangements for alley-gating are undertaken prior to the commencement of the development. It is considered that these measures would ensure that the development would unduly harm the surrounding highway.

Electric vehicle (EV) charging points – As stated a pre-occupation condition has been recommended to ensure that each house has an in-curtilage EV charging point.

Cycle storage – Each house would incorporate a timber cycle storage shed and a condition has been recommended to ensure the implementation of these details as part of the development, thereby contributing to the provision of sustainable transportation options.

Construction Management Plan - Given the relationship of the site to neighbouring houses, it is considered that the undertaking of construction will need to be

appropriately managed. A pre-commencement condition has therefore been recommended to ensure the implementation of agreed details, including: the provision of site fencing; construction traffic management and vehicle routing and measures to control dust and mud on the surrounding public highway. On this basis, it is considered that measures can be put in place to satisfactorily safeguard residential amenity and highway safety.

Waste management - The applicant has demonstrated that the site has the capability and capacity to accommodate waste and recyclable material, which accords to current City Council guidance. The following bins and containers would be contained within the rear gardens and screened from the street by garden fences:

- 1 x mixed waste bin (grey);
- 1 x glass and plastic (green);
- 1 x cardboard / paper (blue);
- 1 x food caddy.

A condition has been recommended to ensure that the development is fully implemented in accordance with the submitted waste management arrangements, including the provision of the bin storage structures prior to the first occupation of the residential units, which shall be maintained in situ thereafter.

Noise - As stated, the site is relatively close to the Etihad and Academy Stadiums, which produce noise during events, which are predictable and not generally reflective of the predominant noise environment, which is predominantly residential and affected by noise from major roads. The applicant has provided supplementary information relating to additional sources that may affect the development. Given the limited sources of such noise, given the predominantly residential nature of the area, it is considered that the concerns of Environmental Health are capable of being addressed.

Air quality - The submitted air quality report identifies, the construction period as the most likely source of air pollution. The implementation of development in accordance with a construction management plan, as required by condition, would incorporate satisfactory air quality controls (during the construction period) thereby reducing this potential harmful impact. The air quality report also considers that the occupation of the proposed houses would have limited scope for traffic generation as the development would be limited to 23 houses. However, to offset any potential future impact on air quality, the applicant has been advised that a pre-occupation condition will be included to ensure the delivery of electric vehicle charging points for each property to an agreed specification. Given, the magnitude of the development, such measures are considered to be appropriate and a proportionate response to the maintenance of air quality.

Land conditions - The applicant has submitted a ground condition survey that has been assessed and gives assurance that issues relating to historic land contamination are capable of being addressed. A condition has been recommended to ensure that submission of additional details pertaining to the report and to ensure the implementation of an appropriate remediation strategy and all necessary mitigation works.

Flood risk and site drainage - The applicant has agreed to a pre-commencement condition relating to the provision of a scheme to be implemented in accordance with measures to secure sustainable site drainage. This condition would also respond to United Utilities request for the inclusion of sustainable site drainage. The applicant has submitted a revised FRA, which has been referred to the Environment Agency. It is also considered that issues relating to the impact of the development on increased flood risk can be addressed.

Tree removal and landscaping – The applicant has submitted a tree report that identifies the removal of 11 trees. As stated the applicant proposes to provide 70 replacement trees and 12 fruit trees. The proposed tree planting would include native and naturalised species and the inclusion of fruit trees being incorporated to aid bio-diversity and to allow residents to harvest resulting fruit. Avenues of trees are proposed to form a pedestrian route the open space and play area and open space. Wildflower planting, mounding and shrub planting would be utilised to provide defensible space to boundaries adjacent to the open space. The applicant has indicated that rear gardens would incorporate paving and grass seeding. Where appropriate retained and new trees would be incorporated into gardens of proposed houses particularly where there would be contribution to the quality of the streetscene. A condition has been recommended to ensure that the submitted detailed landscaping would implemented as part of the development and that it be supplement with details to be agreed in relation to its subsequent on-going maintenance. It is considered that the proposed arrangements would aid to the quality of the development and secure long term management to thereby deliver wider benefits to the amenity of the local community.

Ecology – A condition has been recommended relating the development to the findings of the submitted ecology report that would ensure the provision of:

- i. Bat and bird boxes;
- ii. Replacement tree planting;
- iii. Incorporation of native planting species within the landscaping scheme.

A condition has been included to prevent removal of the tree during bird nesting season. It is therefore considered that the development presents strong opportunities for bio-diversity enhancement.

Conclusion - The development would secure the residential use of a previously developed parcels of land. It is considered that a satisfactory quality of elevational design and the internal space would meet the Council's space criteria. Any potentially harmful impact of the development on residential amenity has been either addressed through the submitted details or is capable of being addressed by condition. The development would positively contribute to the supply of affordable family housing in the local area. The proposed development is therefore considered to be acceptable.

Human Rights Act 1998 considerations

– This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including

local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the approval of the application is proportionate to the wider benefits of approval and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation: MINDED TO APPROVE subject to a s.106 Agreement to secure a retained contribution of affordable housing as part of the authorised development, the Environment Agency being satisfied with the additional flood risk information and no adverse comments being received from the Coal Authority.

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. The applicant's agent engaged in pre-application discussions with officers. On-going dialogue throughout the consideration of the planning application has secured sufficient and satisfactory information that has allowed the planning application to be fully assessed. On balance, it is considered that the development is of a scale and magnitude that would relate positively to the surrounding area, be capable of achieving a high standard of urban design and add to the provision of affordable housing in the city.

Conditions to be attached to the decision

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents (references to be included) and stamped as received by the City Council on: 15 November 2019, 29 November 2019, 9 January 2020 and 31 January 2020:

Planning application forms

Site Location Plan - 1914-GWP-01-01-DR-A-(PA) 0001 Rev P01

Site Topographical Survey - 1914-GWP-01-01-DR-A-(PA) 0002 Rev P01

Proposed Site Plan - 1914-GWP-01-01-DR-A-(PA) 0003 Rev P03
Plot Boundaries and Sectionals - 1914-GWP-01-01-DR-A-(PA)-0004 Rev P03
Footpath Diversions & Closures & Adoptions - 1914-GWP-01-01-DR-A-(PA)-0005 Rev P03
Site Landscaping - 1914-GWP-01-01-DR-A-(PA)-0006 Rev P02
Bike Storage - 1914-GWP-01-01-DR-A-(PA)-0008 Rev P01
External Space / Landscaping - 1914-GWP-01-01-DR-A-(PA)-0009 Rev P03
Boundary Treatment - 1914-GWP-01-01-DR-A-(PA)-0010 Rev P03
Fencing and Brick Wall Details - 1914-GWP-01-01-DR-A-(PA)-0011 Rev P02
Waste Management Strategy - 1914-GWP-01-01-DR-A-(PA)-0012 Rev P03
Proposed Landscape and Urban Realm Improvements - 1914-GWP-01-01-DR-A-(PA)-0013 Rev P01
Street Lighting - 1914-GWP-01-01-DR-A-(PA)-0014 Rev P01
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0021 Rev P02 (1 of 2)
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0022 Rev P02 (2 of 2)
3-D Site View (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0030 Rev P03
Renders (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0031 Rev P03
Architectural Drawings - Block 1 - 1914-GWP-01-01-DR-A-(PA)-0051 Rev P02
Architectural Drawings - Block 2 - 1914-GWP-01-01-DR-A-(PA)-0052 Rev P01
Architectural Drawings - Block 3 - 1914-GWP-01-01-DR-A-(PA)-0053 Rev P01
Architectural Drawings - Block 4 - 1914-GWP-01-01-DR-A-(PA)-0054 Rev P01
Architectural Drawings - Block 5 - 1914-GWP-01-01-DR-A-(PA)-0055 Rev P01
Architectural Drawings - Block 6 - 1914-GWP-01-01-DR-A-(PA)-0056 Rev P01
Architectural Drawings - Block 7 - 1914-GWP-01-01-DR-A-(PA)-0057 Rev P01
Architectural Drawings - Block 8 - 1914-GWP-01-01-DR-A-(PA)-0058 Rev P01
Architectural Drawings - Block 9 - 1914-GWP-01-01-DR-A-(PA)-0059 Rev P01
Architectural Drawings - Block 10 - 1914-GWP-01-01-DR-A-(PA)-0060 Rev P01
Typical 2B/3P House Layout - 1914-GWP-01-01-DR-A-(PA)-0071 Rev P01
Typical 3B/4P House Layout 1914-GWP-01-01-DR-A-(PA)-0072 Rev P01
Response to Planning Comments - (ref 125474/FO/2020) for One Manchester by Eco Res/ GWPA Ref: (299)1914-GWP-01-XX-PP-A-1B-Response Rev P01
Tree Survey and Root Protection Area Ref: 6039a.01B
Mayton Street Tree Survey Report (Revision B) by TBA Landscape Architects dated August 2019 Ref: PD/6039b/TSR/Rev B/AUG19 (Revised November 2019)
Design and Access Statement for One Manchester – Mayton Street by Eco Res/ GWPA Ref: (299)1914-GWP-01-XX-PP-A-1B-D&A Rev P02 dated 27 November 2019
Energy Statement, Mayton, PA1422 Rev A dated November 2019 by TACE and stamped as received by the City Council, as Local Planning Authority, on 15 November 2019
Preliminary Ecological Appraisal at land off Thames Close
Manchester Greater Manchester M11 2FG Ref: UES02679/01 by United Environmental Services Ltd dated 4 September 2019
Air Quality Assessment - Mayton Street, Beswick for One Manchester Reference: 3049-2r1 by Redmore Environmental dated: 30th August 2019
Proposed residential development at Mayton Street in Openshaw, Manchester (23 affordable dwellings) Transport Statement by VTC (Highway & Transportation Consultancy) dated 29 November 2019
Crime Impact Statement: Mayton Street for: GWP Architecture Version A dated 7 November 2019 URN: 2019/0470/CIS/01

Affordable Housing Statement for Mayton Street by One Manchester dated October 2019

Profroma for Planning Applications Waste Management Strategy dated 8 October 2019

Email for GWP Architecture confirming that doors provide the main front and rear access to each of authorised houses will achieve accessible thresholds in accordance with Building Regulations Part M (2) 2.20 d-i.

Letter to GWP Architecture from Druk Ltd relating to acoustic attenuation dated 9 January 2020

Reason - To ensure that the development is carried out in accordance with the approved plans, pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester.

3) Before the commencement of above ground construction works, samples and specifications of all materials to be used on all external elevations of the development and external hard surfaces have been submitted to and approved in writing by the City Council as local planning authority. The development shall be fully implemented in accordance with the approved details, which shall be maintained in situ thereafter.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1, EN1 and DM1 of the Core Strategy for the City of Manchester.

4) The proposed development should be designed and constructed in accordance with the recommendations contained within section 3.3 of document referenced: Crime Impact Statement: Mayton Street for: GWP Architecture Version A dated 7 November 2019 URN: 2019/0470/CIS/01. The development shall only be carried out in accordance with these approved details and occupation or use shall not commence until the City Council as local planning authority has acknowledged, in writing, that it has received written confirmation of 'Secured by Design' accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1, EN1 and DM1 of the Core Strategy for Manchester and to reflect the guidance contained in the National Planning Policy Framework.

5) The planning permission hereby granted relates to the formation of 10 x two bedroom, 3 person houses and 13 x three bedroom, 4 person houses (Class C3) as shown on the approved drawings.

Reason - For the avoidance of doubt and in the interests of residential amenity pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester.

6) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no part of the residential units shall be used for any other purpose (including any other purpose in Class C3 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 as amended by The Town

and Country Planning (Use Classes) (Amendment) (England) Order 2015, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification) other than the purpose(s) of C3(a) not precluding occupation by two unrelated people sharing a property.

Reason - In the exceptional circumstances of a proliferation of HMOs restricting housing choice and adversely affecting sustainability and in the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through the provision of accommodation that is suitable for people living as families pursuant to paragraph 7.4 of the Guide to Development in Manchester: Supplementary Planning Document and Planning Guidance, the National Planning Policy Framework and policies SP1 and DM1 of the Core Strategy for the City of Manchester.

7) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no garages, extensions, porches or outbuildings shall be erected other than those expressly authorised by this permission.

Reason - In the interests of residential amenity pursuant to policy SP1 and DM1 of the Core Strategy for the City of Manchester.

8) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no windows shall be inserted into the elevations of the dwellinghouses hereby approved other than those shown on the approved drawings referenced:

Proposed Site Plan - 1914-GWP-01-01-DR-A-(PA) 0003 Rev P03
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0021 Rev P02 (1 of 2)
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0022 Rev P02 (2 of 2)
3-D Site View (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0030 Rev P03
Renders (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0031 Rev P03
Architectural Drawings - Block 1 - 1914-GWP-01-01-DR-A-(PA)-0051 Rev P02
Architectural Drawings - Block 2 - 1914-GWP-01-01-DR-A-(PA)-0052 Rev P01
Architectural Drawings - Block 3 - 1914-GWP-01-01-DR-A-(PA)-0053 Rev P01
Architectural Drawings - Block 4 - 1914-GWP-01-01-DR-A-(PA)-0054 Rev P01
Architectural Drawings - Block 5 - 1914-GWP-01-01-DR-A-(PA)-0055 Rev P01
Architectural Drawings - Block 6 - 1914-GWP-01-01-DR-A-(PA)-0056 Rev P01
Architectural Drawings - Block 7 - 1914-GWP-01-01-DR-A-(PA)-0057 Rev P01
Architectural Drawings - Block 8 - 1914-GWP-01-01-DR-A-(PA)-0058 Rev P01
Architectural Drawings - Block 9 - 1914-GWP-01-01-DR-A-(PA)-0059 Rev P01
Architectural Drawings - Block 10 - 1914-GWP-01-01-DR-A-(PA)-0060 Rev P01
Typical 2B/3P House Layout - 1914-GWP-01-01-DR-A-(PA)-0071 Rev P01
Typical 3B/4P House Layout 1914-GWP-01-01-DR-A-(PA)-0072 Rev P01

Reason - In the interests of residential amenity pursuant to policy SP1 and DM1 of the Core Strategy for the City of Manchester.

9) Before the occupation of the development windows to bathrooms / wet rooms as so indicated on the approved drawings ref:

Proposed Site Plan - 1914-GWP-01-01-DR-A-(PA) 0003 Rev P03
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0021 Rev P02 (1 of 2)
Contextual Street Elevations - 1914-GWP-01-01-DR-A-(PA)-0022 Rev P02 (2 of 2)
3-D Site View (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0030 Rev P03
Renders (From BIM) - 1914-GWP-01-01-DR-A-(PA)-0031 Rev P03
Architectural Drawings - Block 1 - 1914-GWP-01-01-DR-A-(PA)-0051 Rev P02
Architectural Drawings - Block 2 - 1914-GWP-01-01-DR-A-(PA)-0052 Rev P01
Architectural Drawings - Block 3 - 1914-GWP-01-01-DR-A-(PA)-0053 Rev P01
Architectural Drawings - Block 4 - 1914-GWP-01-01-DR-A-(PA)-0054 Rev P01
Architectural Drawings - Block 5 - 1914-GWP-01-01-DR-A-(PA)-0055 Rev P01
Architectural Drawings - Block 6 - 1914-GWP-01-01-DR-A-(PA)-0056 Rev P01
Architectural Drawings - Block 7 - 1914-GWP-01-01-DR-A-(PA)-0057 Rev P01
Architectural Drawings - Block 8 - 1914-GWP-01-01-DR-A-(PA)-0058 Rev P01
Architectural Drawings - Block 9 - 1914-GWP-01-01-DR-A-(PA)-0059 Rev P01
Architectural Drawings - Block 10 - 1914-GWP-01-01-DR-A-(PA)-0060 Rev P01
Typical 2B/3P House Layout - 1914-GWP-01-01-DR-A-(PA)-0071 Rev P01
Typical 3B/4P House Layout 1914-GWP-01-01-DR-A-(PA)-0072 Rev P01

shall be obscure glazed to a specification of no less than level 5 of the Pilkington Glass Scale or such other alternative equivalent and shall remain so in perpetuity.

Reason - To protect the amenity and living conditions of adjacent residential property from overlooking or perceived overlooking and in accordance with policies SP1 and DM1 of the Core Strategy for the City of Manchester.

10) Before the occupation of the authorised development, full details and specifications of all off-site highways works, including footpath reinstatement to Beede Street, Mayton Street and Herne Street shall be submitted to and approved in writing by the City Council as local planning authority. The required works shall be fully implemented prior to the first occupation of any part of the development and maintained in situ thereafter.

Reason - In the interest of pedestrian and highway safety and in the interests of local amenity, as specified in policies SP1, T2 and DM1 of the Core Strategy for the City of Manchester and guidance contained within the National Planning Policy Framework.

11) Before the occupation of the authorised development, the alley positioned adjacent to the western and northern boundaries of 31 to 47 Herne Street shall be gated in accordance with the details of the drawings referenced: Boundary Treatment - 1914-GWP-01-01-DR-A-(PA)-0010 Rev P03. The development shall be fully implemented prior to the first occupation of any part of the development and maintained in situ thereafter.

Reason - In the interest of pedestrian and highway safety and in the interests of local amenity, as specified in policies SP1, T2 and DM1 of the Core Strategy for the City of Manchester and guidance contained within the National Planning Policy Framework.

12) The development is to be undertaken in accordance with the waste management strategy comprising documents referenced: Planning application - Waste Management Strategy Pro Forma dated 8 October 2019 and Design and Access Statement for One Manchester – Mayton Street by Eco Res/ GWPA Ref: (299)1914-GWP-01-XX-PP-A-1B-D&A Rev P02: Section 5.3 Waste Strategy and drawing referenced: Waste Management Strategy - 1914-GWP-01-01-DR-A-(PA)-0012 Rev P03. The agreed arrangement including the associated bin storage structures shall be implemented in full prior to the first occupation of the authorised houses and maintained in situ thereafter.

Reason - In the interests of residential amenity and to secure appropriate arrangements for the storage and collection of segregated waste and recycling, pursuant to policies SP1, EN19 and DM1 of the Core Strategy for the City of Manchester.

13) Before the occupation of the authorised development, details of the design and appearance of the proposed segregated recycling bin enclosures (as shown on drawing ref: Waste Management Strategy - 1914-GWP-01-01-DR-A-(PA)-0012 Rev P03) shall be submitted to and approved in writing by the City Council and local planning authority. The approved details shall be fully implemented as part of the agreed waste management strategy prior to the occupation of the development and maintained in situ thereafter.

14) Before the occupation of the authorised development, car parking areas shown on drawing Proposed Site Plan - 1914-GWP-01-01-DR-A-(PA) 0003 Rev P03 shall be surfaced and made available for use. The car parking spaces shall be maintained in situ prior to the occupation of the authorised houses and remain in situ thereafter.

Reason - In the interest of residential amenity and pedestrian and highways safety, pursuant to policies SP1, T1, T2 and DM1 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

15) Before the occupation of the authorised development, the cycle storage arrangements as shown on drawings referenced: Proposed Site Plan - 1914-GWP-01-01-DR-A-(PA) 0003 Rev P03 and Bike Storage - 1914-GWP-01-01-DR-A-(PA)-0008 Rev P01 shall be fully implemented and retained in situ thereafter.

Reason - In the interests of residential amenity and to encourage the adoption of sustainable transportation options, pursuant to policies SP1, T1, T2 and DM1 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

16 a) Before the development hereby approved commences, a report (the Preliminary Risk Assessment) to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas relevant to the site shall be submitted to and approved in writing by the City Council as local planning authority. The Preliminary Risk Assessment shall conform to City Council's current guidance document (Planning Guidance in Relation to Ground Contamination).

In the event of the Preliminary Risk Assessment identifying risks which in the written opinion of the Local Planning Authority require further investigation, the development shall not commence until a scheme for the investigation of the site and the identification of remediation measures (the Site Investigation Proposal) has been submitted to and approved in writing by the City Council as local planning authority.

The measures for investigating the site identified in the Site Investigation Proposal shall be carried out, before the development commences and a report prepared outlining what measures, if any, are required to remediate the land (the Site Investigation Report and/or Remediation Strategy) which shall be submitted to and approved in writing by the City Council as local planning authority.

b) When the development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies DM1 and EN18 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

17) Prior to the commencement of the development hereby approved, a detailed construction management plan outlining working practices during development shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include:

- i. Display of an emergency contact number;
- ii. Details of wheel washing;
- iii. Compound locations where relevant;
- iv. Location, removal and recycling of waste;
- v. Routing strategy and swept path analysis;
- vi. Parking of construction vehicles and staff;
- vii. Sheeting over of construction vehicles.

Development shall be carried out in accordance with the approved construction management plan upon commencement of authorised works.

Reason - To safeguard the amenities of nearby residents and highway safety, pursuant to policies SP1, EN19 and DM1 of the Core Strategy for the City of Manchester.

18) No removal or other work to trees and or hedges shall be undertaken in the main bird breeding season (March to July inclusive), unless nesting birds are found to be absent, by a suitably qualified person.

Reason - To safeguard local bio-diversity and nature conservation pursuant to policy EN15 of the Core Strategy for the City of Manchester.

19) Before the occupation of the development, a scheme shall be submitted to and approved in writing by the City Council as local planning authority relating to the provision, position and specification electricity vehicle charging points to each house. The development shall be fully implemented in accordance with the approve scheme prior to the occupation of the authorised houses and maintained in situ thereafter.

Reason - In the interests of residential amenity and the contribute to the reduction of carbon emissions and improve air quality pursuant to policies SP1, EN16 and DM1 of the Core Strategy for the City of Manchester and the National Planning Policy Framework.

20) No development shall take place until surface water drainage works, designed in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards, have been submitted to and approved in writing by the Local Planning Authority.

Reason: To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution, pursuant to policies EN8 and EN14 of the Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

21) The development hereby approved shall be carried out in accordance with document ref: Energy Statement, Mayton, PA1422 Rev A dated November 2019 by TACE and stamped as received by the City Council, as Local Planning Authority, on 15 November 2019 and stamped as received by the City Council, as Local Planning Authority, on the 13 November 2019. A post construction review certificate / statement shall be submitted for approval, within a timescale that has been previously agreed in writing, to the City Council as Local Planning Authority.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1, T2, EN4, EN8 and DM1 of the Core Strategy for the City of Manchester and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

22) Notwithstanding the details of the document ref: Energy Statement, Mayton, PA1422 Rev A dated November 2019 by TACE and Design and Access Statement for One Manchester – Mayton Street by Eco Res/ GWPA Ref: (299)1914-GWP-01-XX-PP-A-1B-D&A Rev P02 dated 27 November 2019 and prior to their installation details of any photovoltaic panels to be fitted to the exterior roof planes of the

authorised houses, as part of the development, shall be submitted to and approved in writing by the City Council as local planning authority. The authorised photovoltaic panels shall be implemented in accordance with the approved details to a timescale agreed as part of the submitted scheme and maintained in situ thereafter.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1, T2, EN4, EN8 and DM1 of the Core Strategy for the City of Manchester and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

23) Notwithstanding the details of drawing ref: Boundary Treatment - 1914-GWP-01-01-DR-A-(PA)-0010 Rev P03 and Fencing and Brick Wall Details - 1914-GWP-01-01-DR-A-(PA)-0010 Rev P02 and before the occupation of the authorised houses comprehensive details relating to the height, specification and appearance of all boundary walls, fences and gates, shall be submitted to and approved in writing by the City Council as local planning authority. The development shall be fully implemented in accordance with the agreed details prior to occupation of the authorised houses and maintained in situ thereafter.

Reason - In order to secure the site boundaries, achieve appropriate noise attenuation and in the interests of residential amenity pursuant to policies SP1 and DM1 of the Core Strategy for the City of Manchester and saved policy DC26 of the Unitary Development Plan for the City of Manchester.

24) Before the occupation of the authorised development, details of the design, specification and appearance of the three park benches as shown on drawing referenced: Proposed Landscape and Urban Realm Improvements - 1914-GWP-01-01-DR-A-(PA)-0013 Rev P01. The development shall be fully implemented in accordance with the agreed details prior to occupation of the authorised houses and maintained in situ thereafter.

Reason – In the interest of residential amenity pursuant to SP1 and DM1 of the Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

25) Before the occupation of the authorised development, details of the appearance and specification of the lighting columns, as shown on drawing referenced Street Lighting - 1914-GWP-01-01-DR-A-(PA)-0014 Rev P01 shall be submitted to and approved in writing by the City Council as local planning authority. The approved scheme shall be implemented in full to timescale agreed in writing by the City Council as local planning authority and maintained in situ thereafter.

Reason – In the interest of residential amenity pursuant to SP1 and DM1 of the Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

26) Before the occupation of the authorised development, a scheme relating to the maintenance of street furniture and lighting, as detailed on approved drawings:

Proposed Landscape and Urban Realm Improvements - 1914-GWP-01-01-DR-A-(PA)-0013 Rev P01

Site Landscaping - 1914-GWP-01-01-DR-A-(PA)-0006 Rev P02
Street Lighting - 1914-GWP-01-01-DR-A-(PA)-0014 Rev P01

shall be submitted to and approved in writing by the City Council as local planning authority. The approved scheme shall be implemented in full to timescale agreed in writing by the City Council as local planning authority and maintained in situ thereafter.

Reason – In the interest of residential amenity pursuant to SP1 and DM1 of the Core Strategy for the City of Manchester and the guidance within the National Planning Policy Framework.

27) Before the occupation of the authorised development, a scheme shall be submitted to and approved in writing by the City Council as local planning authority, relating to the maintenance of the hereby approved open space, footpaths, tree planting and landscaping details to the communal open space as specified on drawing referenced:

Site Landscaping - 1914-GWP-01-01-DR-A-(PA)-0006 Rev P02
Bike Storage - 1914-GWP-01-01-DR-A-(PA)-0008 Rev P01
External Space / Landscaping - 1914-GWP-01-01-DR-A-(PA)-0009 Rev P03

The approved landscaping scheme shall be fully implemented during the first full planting scheme following the occupation of the development and maintained in situ thereafter in accordance with the approved landscape maintenance scheme. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy for the City of Manchester.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 125474/FO/2019 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

The Coal Authority
Highway Services
Environmental Health

Neighbourhood Team Leader (Arboriculture)
Corporate Property
Parks & Events
MCC Flood Risk Management
Greater Manchester Police
United Utilities Water PLC
Environment Agency
Greater Manchester Ecology Unit

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

A petition signed by 17 residents
6 Rook Street Manchester
5 Flavian Walk Manchester

Relevant Contact Officer : Carl Glennon
Telephone number : 0161 234 4530
Email : c.glennon@manchester.gov.uk

